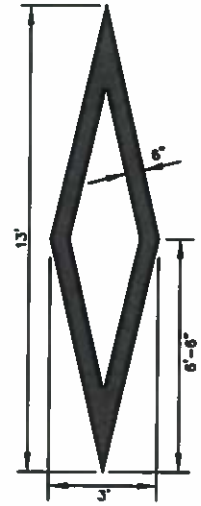
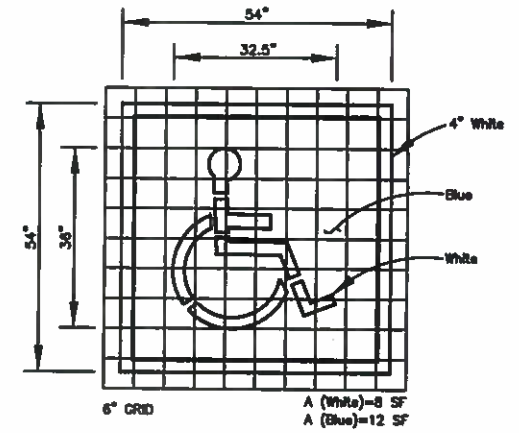


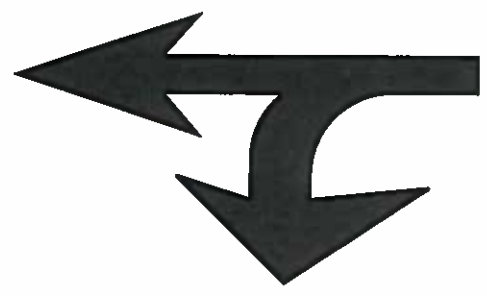
A=42 SF
RIGHT LANE DROP ARROW
 (For a left lane drop condition,
 use the mirror image.)



A=11 SF
PREFERENTIAL
LANE SYMBOL



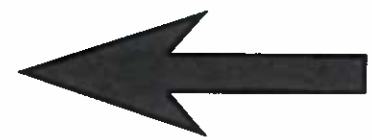
HANDICAPPED PARKING SYMBOL



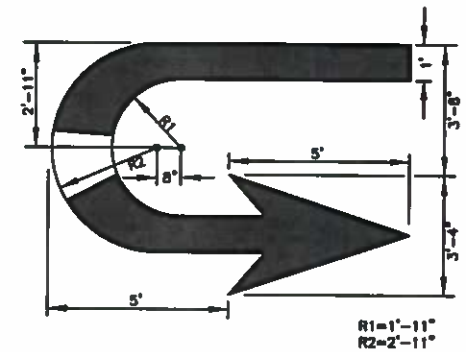
A=29 SF
THRU/LEFT TURN ARROW
 (For the thru/right turn arrow,
 use the mirror image.)



A=16 SF
LEFT/RIGHT TURN ARROW
 (For the right turn arrow,
 use the mirror image.)



A=12 SF
THRU ARROW



A=29 SF
U-TURN ARROW

- NOTES:**
- FOR PAVEMENT MARKING DIMENSIONS NOT DETAILED ABOVE, SEE THE MOST RECENT VERSION OF THE "STANDARD HIGHWAY SIGNS" BOOK.
 - MINOR VARIATIONS IN DIMENSIONS MAY BE ACCEPTED BY THE ACHD TRAFFIC ENGINEER.

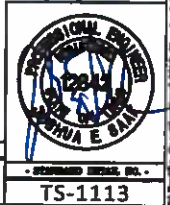
12/4/2014

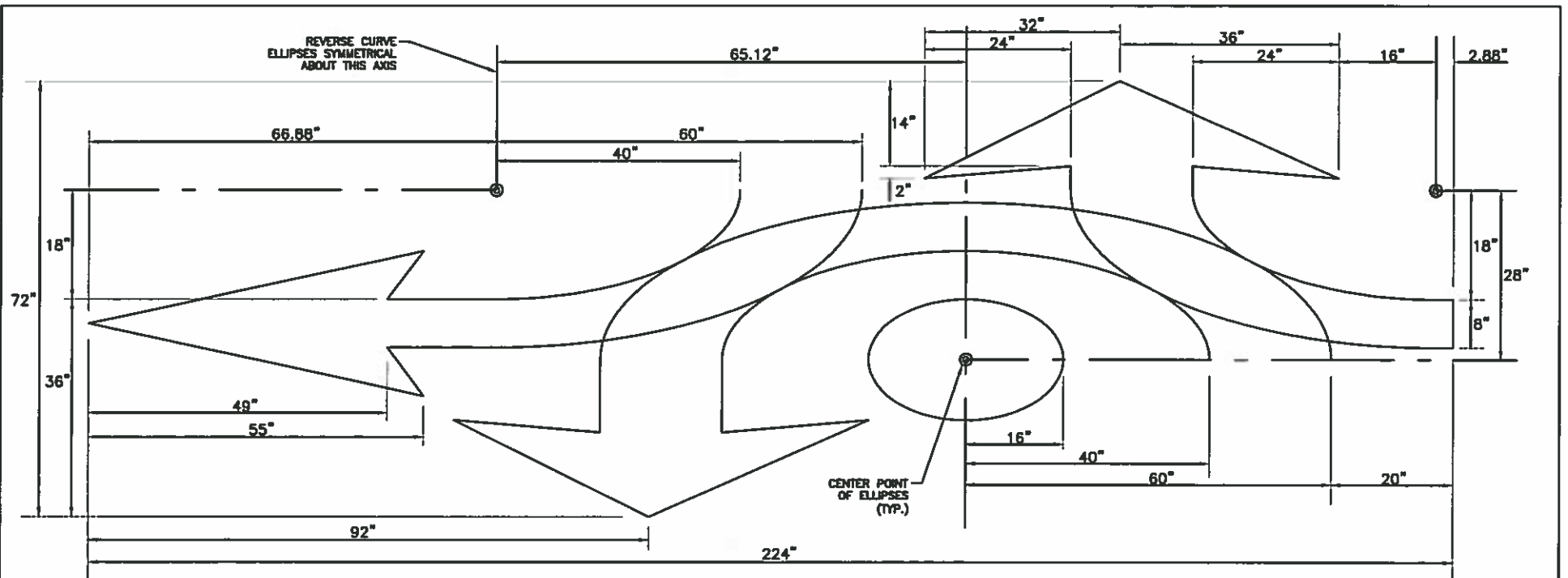
| | | | |
|---|-------|-----|---|
| ▲ | | | |
| ▲ | | | |
| ▲ | 12/04 | ADD | Added u-turn, preferential lane symbols |
| ▲ | 06/09 | BY | |



| | | | |
|-----------------------|--------------|-------------|--------------|
| • SIGNATURES • | | | |
| Drawn By | Justin Smith | Checked By | Justin Smith |
| Date | 12/1/14 | Design By | Justin Smith |
| Scale | as-1110a.000 | Approved By | Steve Green |

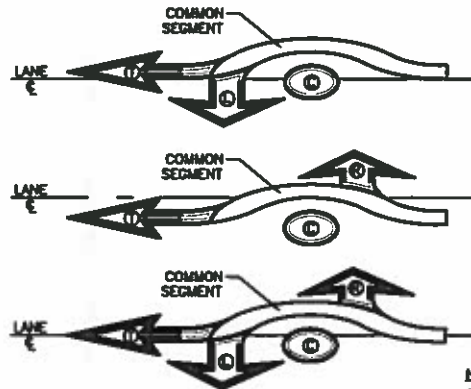
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| • SHEET TITLE • | |
| Standard Lane Use Pavement Marking Details | |



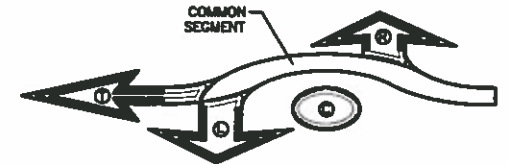


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|-----------|-----------|-----------|
| | | |
| A=28.7 SF | A=23.1 SF | A=24.2 SF |
| | | |
| A=18.6 SF | A=25.2 SF | A=19.6 SF |
| | | |
| A=20.7 SF | A=15.1 SF | A=18.1 SF |

ROUNDBOUT TRAFFIC ARROWS



ARROW PLACEMENT



NOTE:
 LABELED AREAS CORRESPOND TO THE PORTIONS NEEDED FOR EACH TYPE OF ROUNDABOUT MOVEMENT. REFER TO THE PLAN SHEETS FOR TYPE OF ARROW TO BE APPLIED.

COMPONENT KEY 11/26/2014

NOTE:
 ARROW SHOULD BE CENTERED IN THE LANE USING THE LATERAL EXTREMITIES OF THAT ARROW TYPE. MINOR VARIATIONS MAY BE ACCEPTED BY THE ACHD TRAFFIC ENGINEER.

| NO. | DATE | BY | REVISIONS |
|-----|------|----|-----------|
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| DESIGNER | | CHECKER | |
|----------------------|-------------------------|--------------------------|------------------------|
| Drawn By: STV | Design By: Jordan Shult | Checked By: Jordan Shult | Approved By: Tom Green |
| Date: 11/3/13 | Scale: 1/8"=1'-0" | | |
| Title: 66-111(2).DWG | | | |

| PROJECT TITLE | |
|-------------------|--|
| Standard Lane Use | Pavement Marking Details - Roundabouts |



NOTES:

1. IF A MESSAGE CONSISTS OF MORE THAN ONE WORD, IT SHOULD READ "UP", I.E., THE FIRST WORD SHOULD BE NEAREST THE DRIVER.
2. THE SPACE BETWEEN WORDS SHOULD BE AT LEAST FOUR TIMES THE HEIGHT OF THE CHARACTERS FOR LOW SPEED ROADS, BUT NOT MORE THAN TEN TIMES THE HEIGHT OF THE CHARACTERS. THE SPACE MAY BE REDUCED APPROPRIATELY WHERE THERE IS LIMITED SPACE BECAUSE OF LOCAL CONDITIONS.
3. MINOR VARIATIONS IN DIMENSIONS MAY BE ACCEPTED BY THE ENGINEER
4. PORTIONS OF A LETTER, NUMBER OR SYMBOL MAY BE SEPARATED BY CONNECTING SEGMENTS NOT TO EXCEED 2" IN WIDTH.

TYPICAL WORD MARKINGS

(AREA CALCULATED FOR TYPICAL WORK MARKINGS INSTALLED IN THE TRAVEL WAY)

| Text | Area (SF) |
|----------|-----------|
| 25 | 18 |
| 35 | 17 |
| 40 | 20 |
| AHEAD | 31 |
| BIKE | 21 |
| BUS | 20 |
| CAR | 17 |
| CLEAR | 27 |
| HERE | 26 |
| KEEP | 24 |
| LANE | 24 |
| LEFT | 19 |
| NO | 5 |
| PED | 18 |
| RIGHT | 26 |
| SCHOOL | 35 |
| SIGNAL | 32 |
| SLOW | 23 |
| TURN | 24 |
| VEHICLES | 42 |
| WAIT | 19 |
| XING | 21 |

ONLY

A=22 SF

YIELD

A=24 SF

STOP

A=22 SF

11/26/2014



| NO. | DATE | BY | DESCRIPTION |
|-----|------|----|-------------|
| | | | |
| | | | |
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| Scale: N/A | Drawn By: |
| Date: 04/24/07 | Design By: |
| File: 10-1110A.PDW | Approved By: Jim Linton |

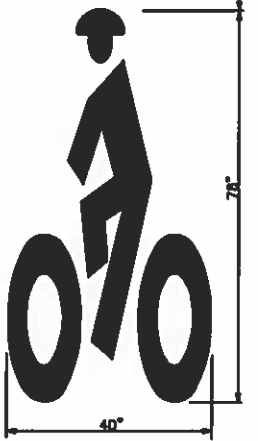
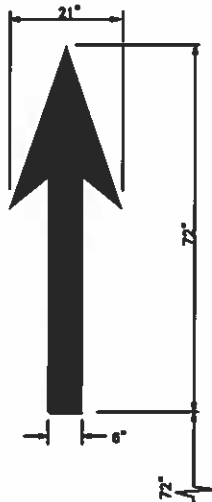
• SIGNATURES •

• BEST TITLE •

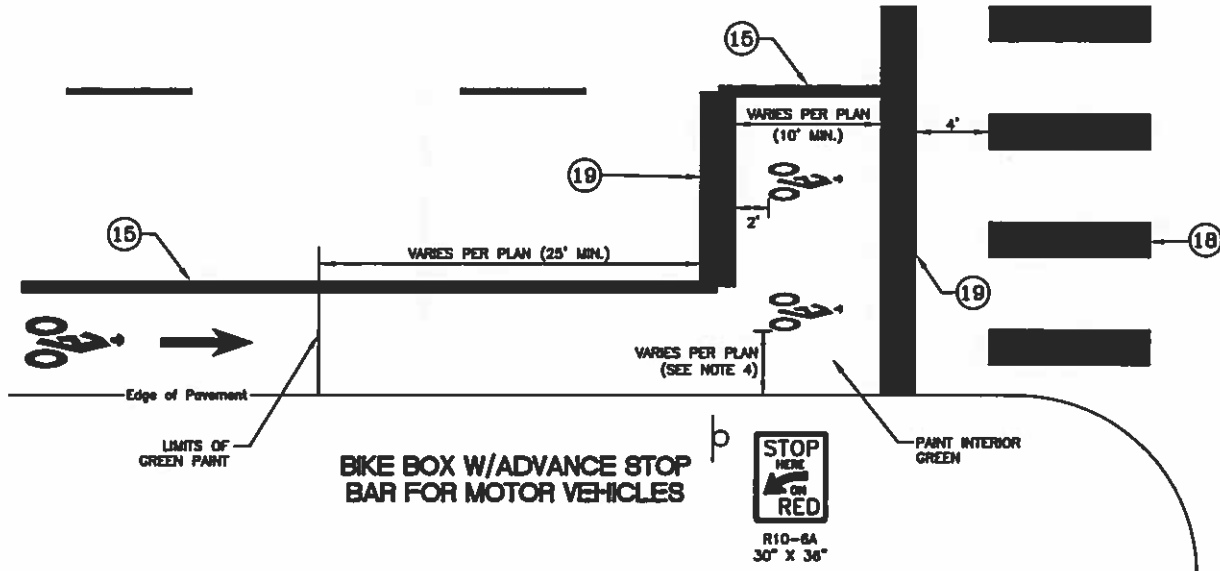
Standard Lane Use
Pavement Marking Details

• STANDARD DRAWING NO. •
TS-1113

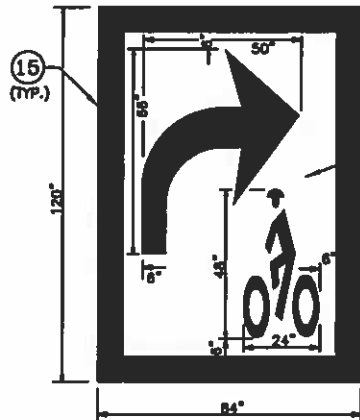
BIKE LANE ARROW A=4 SF



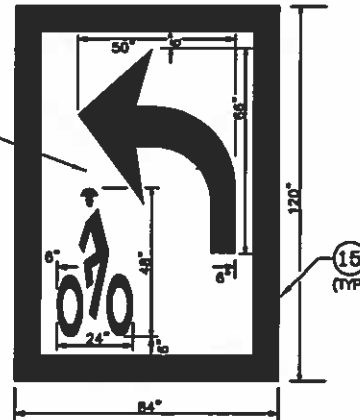
BIKE LANE SYMBOL A=7 SF



BIKE BOX W/ADVANCE STOP BAR FOR MOTOR VEHICLES



RIGHT TURN BIKE QUEUE BOX A=31 SF (BORDER AND STENCILS)



LEFT TURN BIKE QUEUE BOX A=31 SF (BORDER AND STENCILS)

NOTES:

1. BIKE LANE SYMBOLS AND ARROWS SHALL ONLY BE INSTALLED ON ROADWAYS WHERE SUFFICIENT WIDTH IS PROVIDED FOR A BIKE FACILITY, PER THE MOST RECENTLY ADOPTED VERSION OF THE MUTCD. BIKE LANE SYMBOLS AND ARROWS SHALL NOT BE INSTALLED IN VEHICULAR TRAVEL LANES.
2. MINOR VARIATIONS IN DIMENSIONS MAY BE ACCEPTED BY THE ENGINEER.
3. BIKE BOXES ARE CURRENTLY EXPERIMENTAL TREATMENTS AND SHALL ONLY BE INSTALLED IN LOCATIONS DIRECTED BY THE ACHD TRAFFIC ENGINEER.
4. LATERAL DISTANCE OF BIKE SYMBOLS FROM THE EDGE OF TRAVEL WAY WILL VARY BASED ON LANE WIDTHS. CARE SHOULD BE TAKEN TO INSTALL BIKE SYMBOLS TO AVOID VEHICULAR WHEEL PATHS.
5. A MINIMUM OF TWO BIKE SYMBOLS SHALL BE INSTALLED IN A BIKE BOX WITH AN ADVANCE STOP BAR. ADDITIONAL SYMBOLS MAY BE INSTALLED, PER PLAN, OR WITH THE APPROVAL OF THE ACHD TRAFFIC ENGINEER.

11/26/2014



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| DATE | BY | DESCRIPTION |
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| Author: JFB | Drawn By: Joshua Booth |
| Date: 11/26/14 | Design By: Joshua Booth |
| File: 10-1114L200 | Approved By: Russ Green |

STREET TILES
Standard Lane Use
Pavement Marking Details

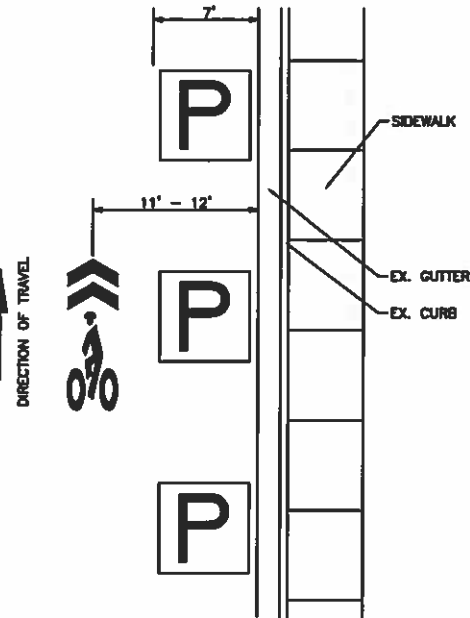
TS-1113



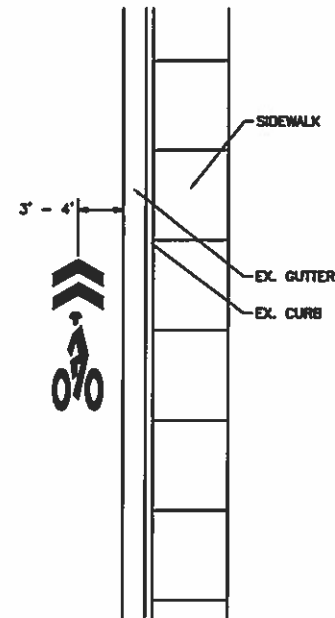
Shared Lane Marking ('Sharrow') Details

NOTES:

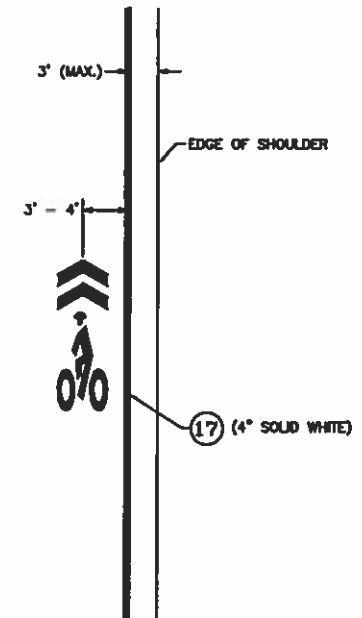
1. WHEN USED, "SHARROWS" SHOULD BE PLACED IMMEDIATELY AFTER PUBLIC STREET INTERSECTIONS AND SPACED AT INTERVALS NOT GREATER THAN 250' THEREAFTER.
2. WHEN USED IN A SHARED LANE WITH ON-STREET PARKING, "SHARROWS" SHOULD BE PLACED SO THAT THE CENTERS OF THE MARKINGS ARE 11' TO 12' FROM THE EDGE OF PAVEMENT.
3. WHEN USED IN A SHARED LANE WITH NO ON-STREET PARKING, "SHARROWS" SHOULD BE PLACED SO THAT THE CENTERS OF THE MARKINGS ARE 3' TO 4' FROM THE EDGE OF PAVEMENT.
4. IN THE ABSENCE OF CURB AND GUTTER, "SHARROWS" SHOULD BE PLACED 3' TO 4' FROM THE EDGELINE MARKING.
5. "SHARROWS" SHALL NOT BE INSTALLED ON ROADWAYS WITH A SPEED LIMIT GREATER THAN 30 MPH.
6. "SHARROWS" SHALL NOT BE INSTALLED IN BIKE LANES, SHOULDERS OR LOCATIONS WHERE SHOULDERS ARE CONSISTENTLY 3' OR GREATER IN WIDTH.
7. SIGNIFICANT VARIATIONS TO "SHARROW" PLACEMENT OUTLINED ABOVE SHALL BE APPROVED BY THE ACHD TRAFFIC ENGINEER PRIOR TO INSTALLATION.



Shared Lane Marking ('Sharrow') Placement With On-Street Parking, Curb and Gutter



Shared Lane Marking ('Sharrow') Placement With No On-Street Parking, Curb and Gutter



Shared Lane Marking ('Sharrow') Placement With No On-Street Parking, No Curb and Gutter

11/26/2014



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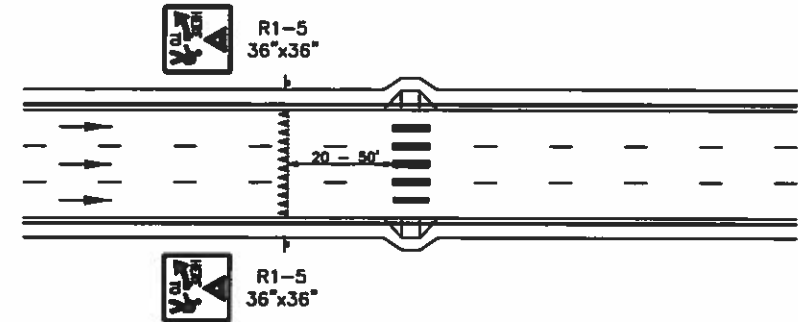
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| • SIGNATURES • | | | |
| Drawn By: <i>[Signature]</i> | Checked By: <i>[Signature]</i> | Design By: <i>[Signature]</i> | Approved By: <i>[Signature]</i> |
| Date: 11/18/14 | Scale: 1"=10' | File: 10-1110-000 | |

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| • SHEET TITLE • | |
| Standard Lane Use | |
| Pavement Marking Details - Shared Lane Markings | |

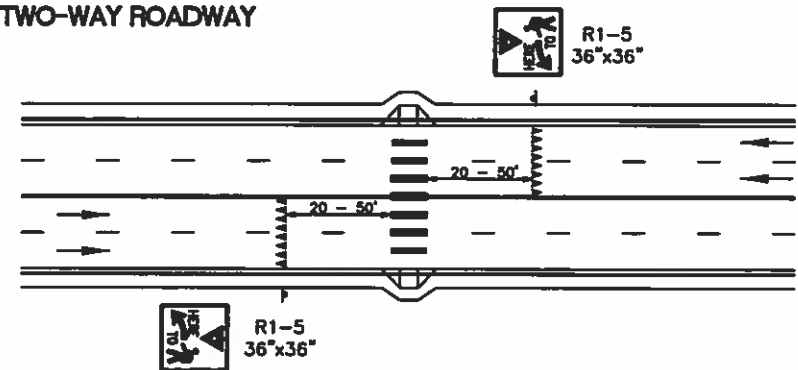
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| • STANDARD SHEET NO. • |
| TS-1113 |

EXAMPLES OF YIELD LINES AT UNSIGNALIZED MIDBLOCK CROSSINGS

A. ONE-WAY ROADWAY



B. TWO-WAY ROADWAY



NOTES:

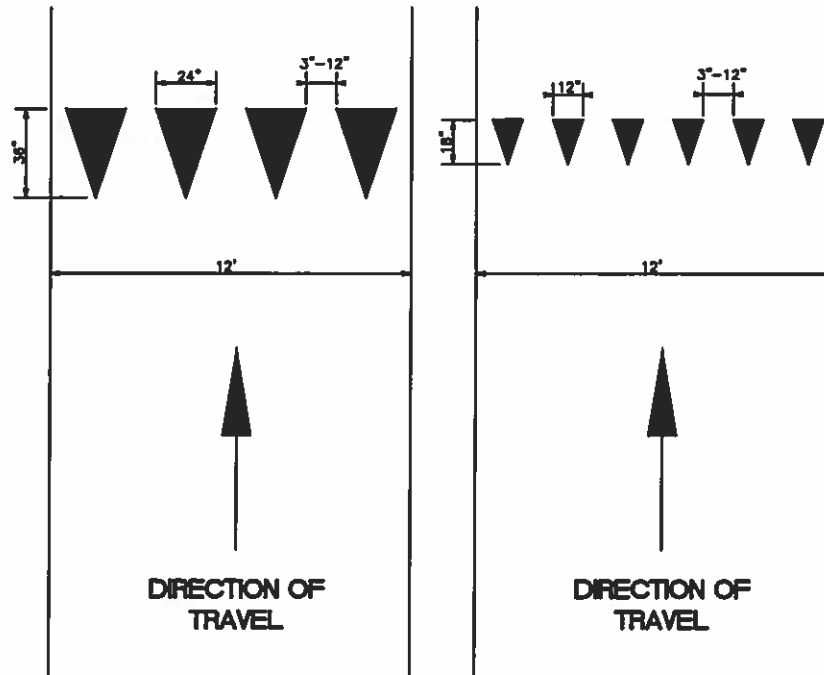
1. IF "STOP HERE FOR PEDESTRIANS" SIGNS ARE USED INSTEAD OF "YIELD HERE TO PEDESTRIANS" SIGNS, STOP BARS SHALL BE USED INSTEAD OF YIELD MARKINGS.
2. CROSSWALK WIDTH SHALL BE 6', 9' OR 12', PER THE DIRECTION OF THE ACHD TRAFFIC ENGINEER.

11/26/2014



TS-1113

Sheet 6 of 12



YIELD LINE LAYOUT
POSTED SPEED >25 MPH
A=3 SF (PER TRIANGLE)

YIELD LINE LAYOUT
POSTED SPEED <=25 MPH
A=0.75 SF (PER TRIANGLE)

NOTES:

1. TRIANGLE HEIGHT IS EQUAL TO 1.5 TIMES THE BASE DIMENSION.
2. TRIANGLE SPACING MAY NEED TO BE ADJUSTED BASED ON THE WIDTH OF THE TRAVEL LANE.
3. 18" YIELD LINE MARKINGS SHALL ONLY BE INSTALLED WITH THE PERMISSION OF THE ACHD TRAFFIC ENGINEER.

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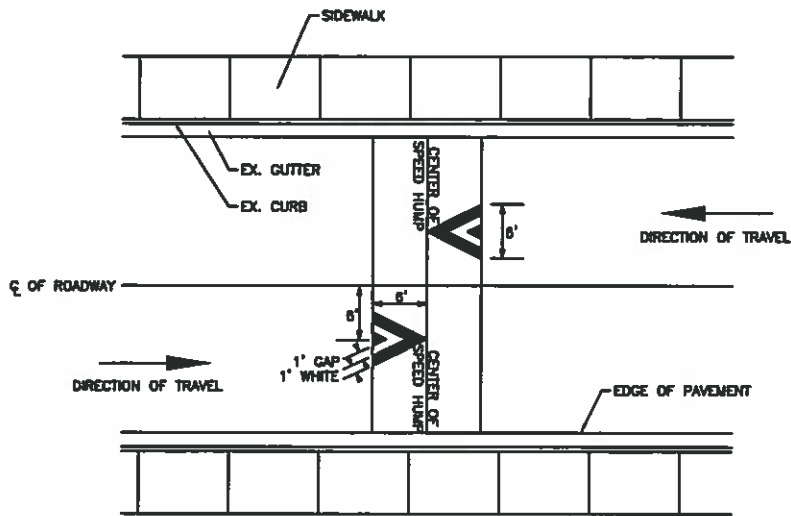


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| Drawn By: Joshua Stark | Checked By: Joshua Stark |
| Design By: Joshua Stark | Approved By: Ross Open |
| Date: 07/18/10 | File: 10-11131200 |

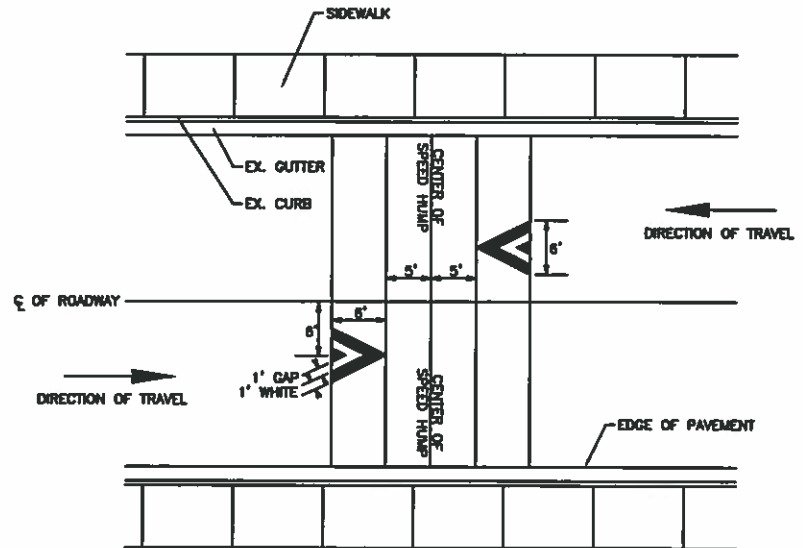
Standard Lane Use
Pavement Marking Details - Yield Line Markings

Ada County Highway District

8776 Adams Street, Boise, Idaho, 83714
www.achd.idaho.gov



**SPEED HUMP MARKINGS LAYOUT
LOCAL ROAD**



**SPEED HUMP MARKINGS LAYOUT
COLLECTOR ROAD**

NOTES:

1. ADVANCE SPEED HUMP MARKINGS MAY BE USED IN ADVANCE OF SPEED HUMPS WHERE ADDED VISIBILITY IS DESIRED. ALL ADVANCE STRIPING SHOULD BE IN ACCORDANCE WITH THE MUTCD.
2. APPROPRIATE ADVANCE WARNING SIGNS MAY BE USED WITH ALL SPEED HUMP INSTALLATIONS.

11/26/2014



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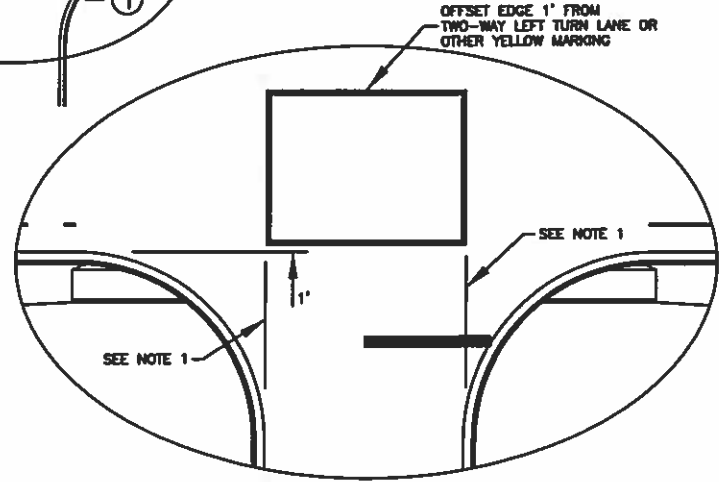
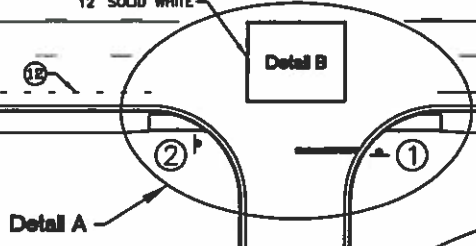
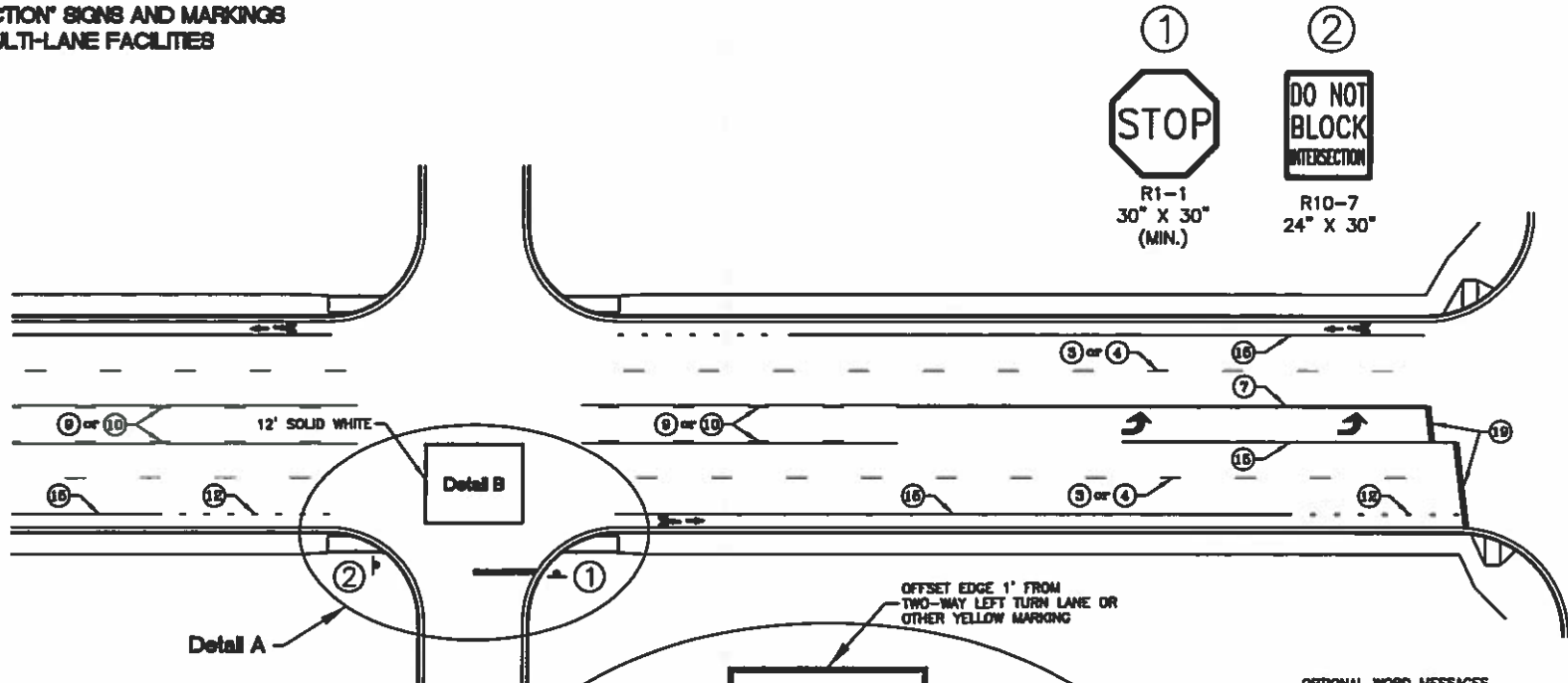


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| Scale: 3/8" | Drawn By: Shawn Bank |
| Date: 07/28/08 | Design By: Shawn Bank |
| File: 11-1113-200 | Approved By: Russ Oyer |

PROJECT TITLE:
Standard Lane Use
Pavement Marking Details - Speed Humps

STANDARD SERIAL NO.:
TS-1113
Sheet 7 of 12

**TYPICAL INSTALLATION OF 'DO NOT BLOCK INTERSECTION' SIGNS AND MARKINGS
MULTI-LANE FACILITIES**



OPTIONAL WORD MESSAGES
(SEE NOTE 4):



Detail B

NOTES:

1. THE EDGELINES OF THE "DO NOT BLOCK..." MARKINGS SHALL ALIGN WITH THE SIDE STREET EDGES OF PAVEMENT. SIGNIFICANT DEVIATION FROM THIS STANDARD SHALL BE APPROVED BY THE TRAFFIC ENGINEER.
2. AN ENGINEERING STUDY SHOULD BE PERFORMED PRIOR TO INSTALLATION OF "DO NOT BLOCK INTERSECTION" SIGNS AND MARKINGS WITH THE APPROVAL OF THE TRAFFIC ENGINEER.
3. MARKED CROSSWALKS ON "DETAIL A" ARE NOT SHOWN FOR CLARITY PURPOSES.
4. WORD MESSAGES SUCH AS "DO NOT BLOCK" OR "KEEP CLEAR" MAY BE INSTALLED WITHIN THE SOLID WHITE LINES AS ADDED EMPHASIS TO DEFINE THE AREA THAT VEHICLES MUST NOT BLOCK.

4/23/2015

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|---------|----|---------------------------|
| DATE | BY | REVISION |
| 4/14/15 | BB | Show design modifications |
| 4/28/15 | BY | REVISION |

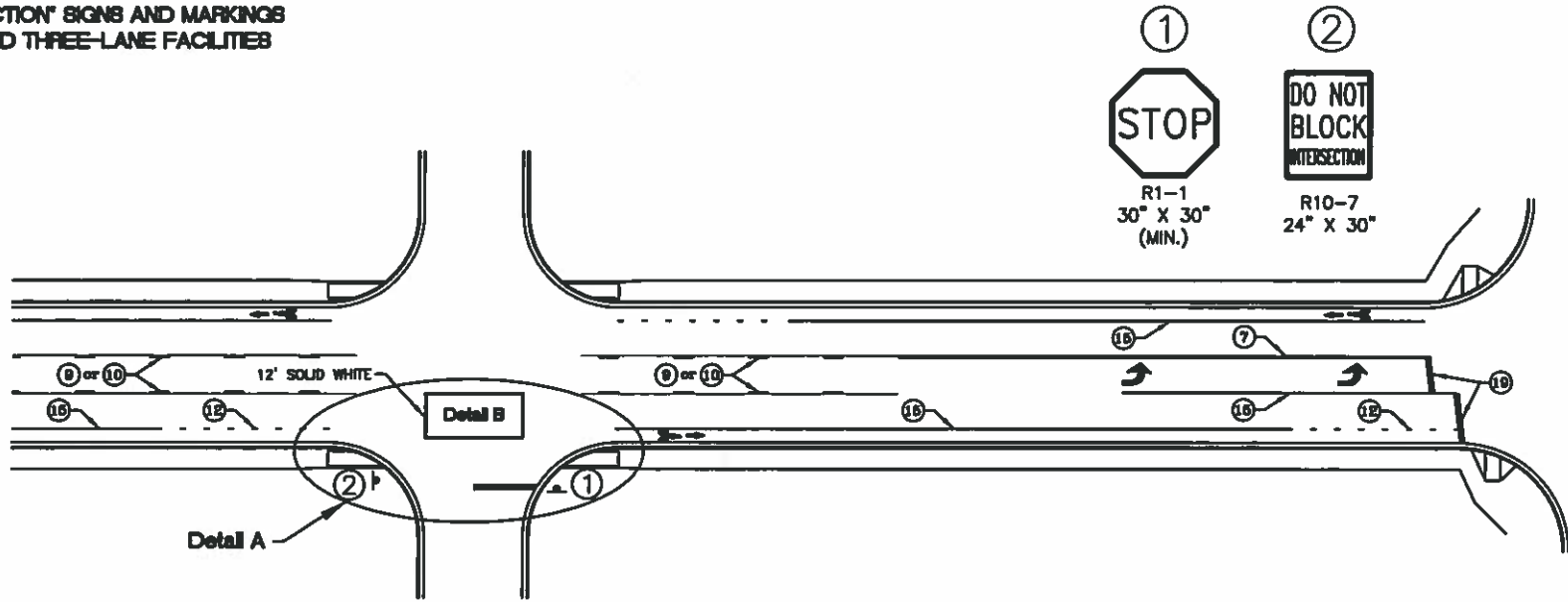


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|------------|-------------|
| Drawn By | Justin Reed |
| Checked By | Justin Reed |
| File | TS-1113.rvt |

Do Not Block Intersection
Standard Detail - Multi-Lane Facilities

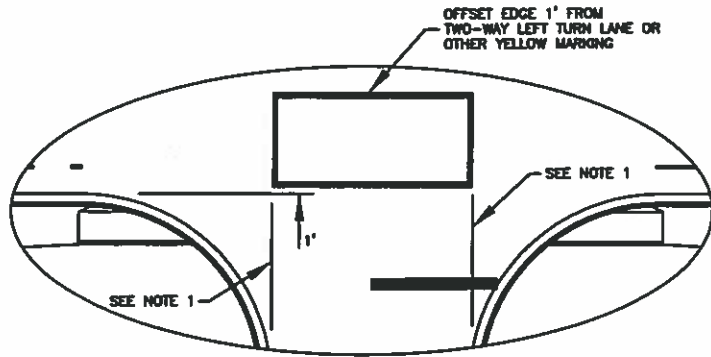


**TYPICAL INSTALLATION OF 'DO NOT BLOCK INTERSECTION' SIGNS AND MARKINGS
TWO AND THREE-LANE FACILITIES**



NOTES:

1. THE EDGELINES OF THE "DO NOT BLOCK..." MARKINGS SHALL ALIGN WITH THE SIDE STREET EDGES OF PAVEMENT. SIGNIFICANT DEVIATION FROM THIS STANDARD SHALL BE APPROVED BY THE TRAFFIC ENGINEER.
2. AN ENGINEERING STUDY SHOULD BE PERFORMED PRIOR TO INSTALLATION OF "DO NOT BLOCK INTERSECTION" SIGNS AND MARKINGS WITH THE APPROVAL OF THE TRAFFIC ENGINEER.
3. MARKED CROSSWALKS ON "DETAIL A" ARE NOT SHOWN FOR CLARITY PURPOSES.
4. WORD MESSAGES SUCH AS "DO NOT BLOCK" OR "KEEP CLEAR" MAY BE INSTALLED WITHIN THE SOLID WHITE LINES AS ADDED EMPHASIS TO DEFINE THE AREA THAT VEHICLES MUST NOT BLOCK.



OPTIONAL WORD MESSAGES
(SEE NOTE 4):



Detail B

Detail A

4/23/2015

| | | | |
|---|------|----|----------------------------|
| ▲ | | | |
| ▲ | | | |
| ▲ | 4/22 | SB | Minor design modifications |
| ▲ | 3/25 | BT | DESIGN |

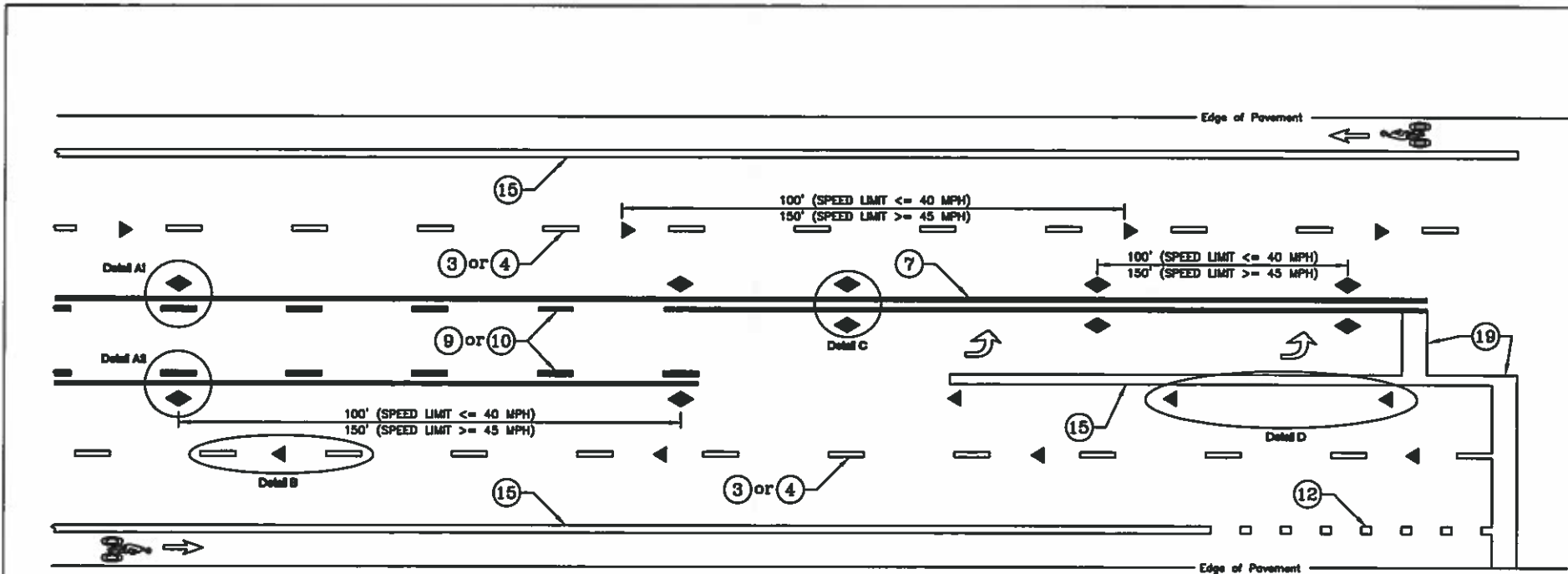


| SIGNATURES | | BEST TITLE | |
|------------|--------------|-------------|--------------|
| Drawn By | Autumn Blank | Design By | Autumn Blank |
| Checker | 4/22/15 | Approved By | Steve Owen |
| File | 15-1118-200 | | |

**Do Not Block Intersection
Standard Detail - Two and Three-Lane Facilities**

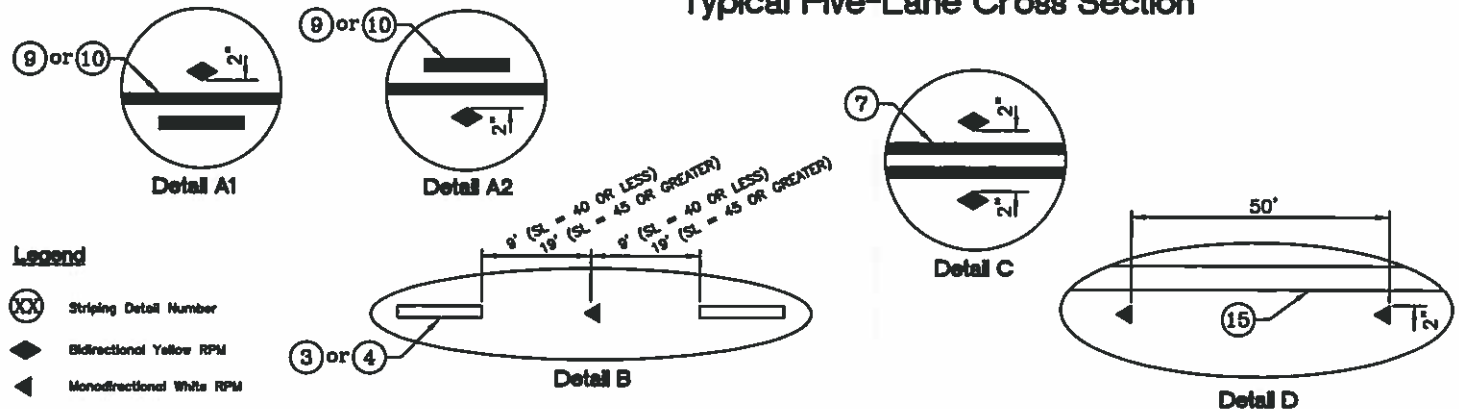


STEVE OWEN, P.E.
TS-1113



Typical Five-Lane Cross Section

- NOTES:**
- SEE ACHD STANDARD DETAIL TS-1113 FOR STANDARD LANE USE PAVEMENT MARKING DETAILS.
 - RPMs SHOULD BE ALIGNED SUCH THAT THE REFLECTIVE ELEMENT IS PERPENDICULAR TO THE DIRECTION OF TRAVEL.
 - THE CENTERLINE OF WHITE RPMs SHALL BE IN LINE WITH THE CENTERLINE OF ALL WHITE BROKEN LANE LINE MARKINGS.
 - IN CASES WHERE THE PREFERRED RPM LOCATION IS NOT PRACTICAL DUE TO CONSTRUCTION JOINT SPACING, LONGITUDINAL RPM SPACING MAY DEVIATE BY NOT MORE THAN 10% FROM THE TYPICAL SPACING. NO DEVIATION FROM THE LATERAL PLACEMENT IS PERMITTED EXCEPT AS APPROVED BY THE ENGINEER.



Legend

- (XX) Striping Detail Number
- ◆ Bidirectional Yellow RPM
- ◀ Monodirectional White RPM

| | | | |
|---|---------|-----|--------------------------------|
| ▲ | | | |
| ▲ | | | |
| ▲ | 4/13 | JES | Minor revisions to RPM spacing |
| ▲ | 2/25/20 | BT | |

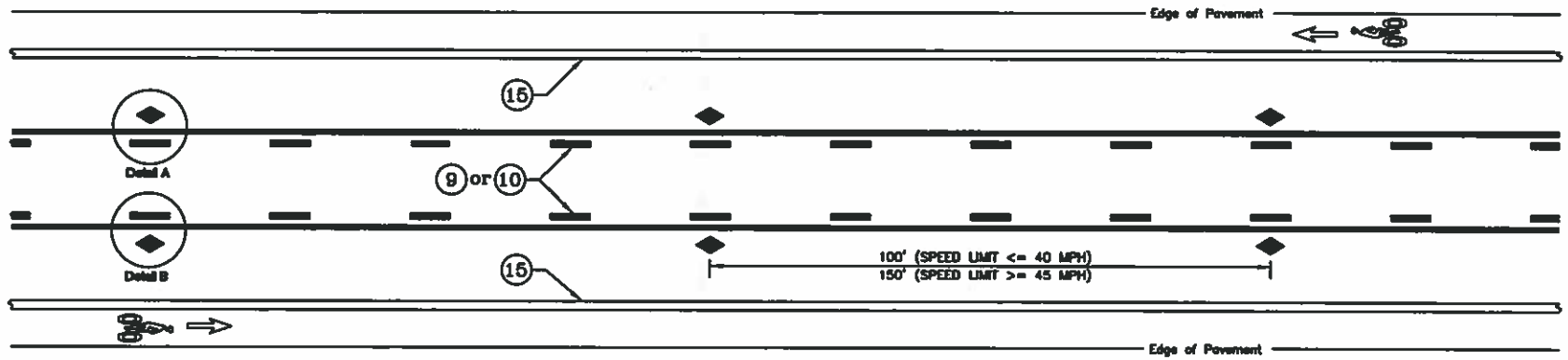


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| Drawn By: Jordon Bank | Checked By: Jordon Bank |
| Date: 06/06/18 | Checked By: Jordon Bank |
| File: 18-1114-000 | Approved By: Steve Strick |

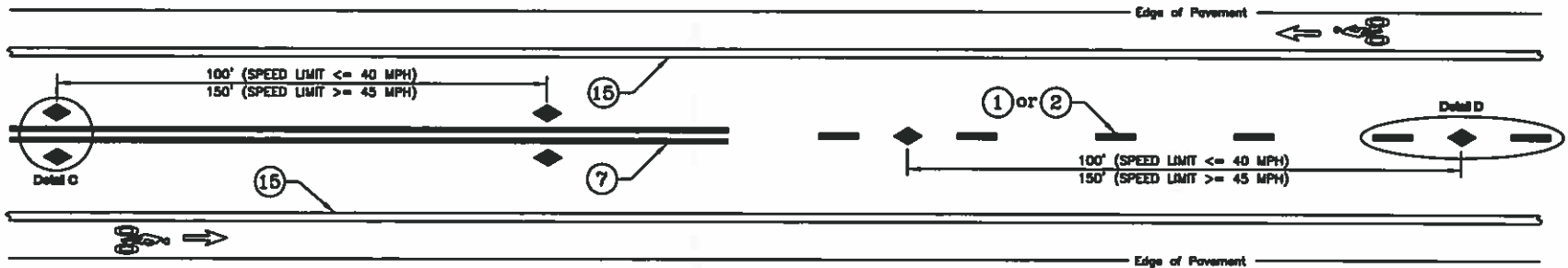
Standard Lane Use Pavement Marking Details - Reflective Pavement Markers (RPMs) - Five Lane Facilities

11/26/2014

STEVE E. HILL
12842
PROFESSIONAL ENGINEER
STATE OF IDAHO



Typical Three-Lane Cross Section

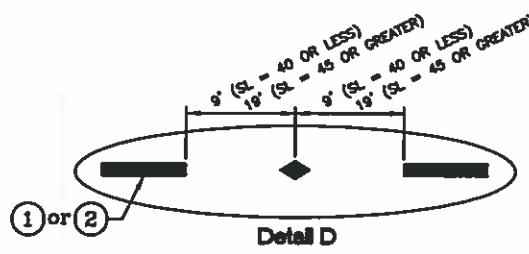
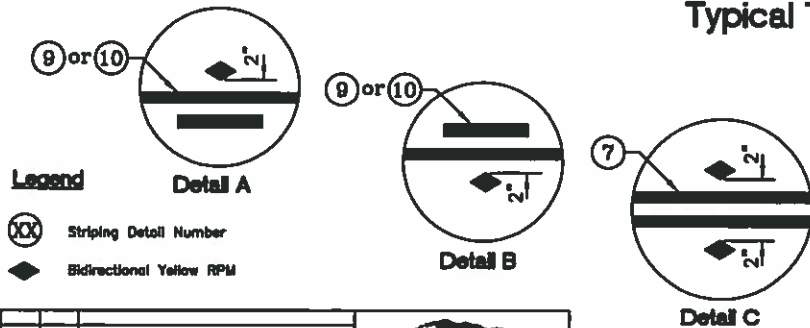


Typical Two-Lane Cross Section

NOTES:

- SEE ACHD STANDARD DETAIL TS-1113 FOR STANDARD LANE USE PAVEMENT MARKING DETAILS.
- RPMs SHOULD BE ALIGNED SUCH THAT THE REFLECTIVE ELEMENT IS PERPENDICULAR TO THE DIRECTION OF TRAVEL.
- THE CENTERLINE OF RPMs SHALL BE IN LINE WITH THE CENTERLINE OF ALL BROKEN LANE LINE MARKINGS.
- IN CASES WHERE THE PREFERRED RPM LOCATION IS NOT PRACTICAL DUE TO CONSTRUCTION JOINT SPACING, LONGITUDINAL RPM SPACING MAY DEVIATE BY NOT MORE THAN 10% FROM THE TYPICAL SPACING. NO DEVIATION FROM THE LATERAL PLACEMENT IS PERMITTED EXCEPT AS APPROVED BY THE ENGINEER.

11/26/2014



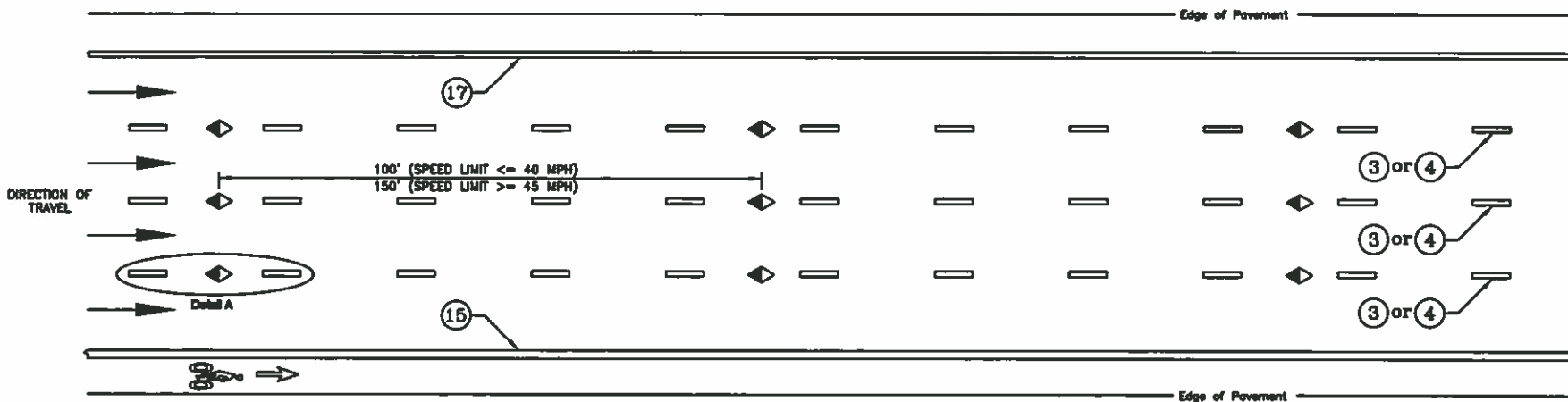
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|----------|------|--------------------------------|
| 4/13 | 2013 | Minor revisions to RPM spacing |
| 02/10/09 | 07 | Revised |



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| Scale: 1/2" | Drawn By: Joshua Smith |
| Date: 05/05/10 | Check By: Joshua Smith |
| File: ts-1113-000 | Approved By: Sharon Martin |

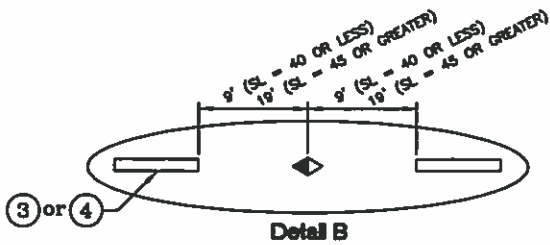
Standard Lane Use Pavement Marking Details - Reflective Pavement Markers (RPMs) - Two and Three Lane Facilities

TS-1113



Typical One Way Street Cross Section

- NOTES:**
- SEE ACHD STANDARD DETAIL TS-1113 FOR STANDARD LANE USE PAVEMENT MARKING DETAILS.
 - RPMs SHOULD BE ALIGNED SUCH THAT THE REFLECTIVE ELEMENT IS PERPENDICULAR TO THE DIRECTION OF TRAVEL.
 - THE CENTERLINE OF RPMs SHALL BE IN LINE WITH THE CENTERLINE OF ALL WHITE BROKEN LANE LINE MARKINGS.
 - IN CASES WHERE THE PREFERRED RPM LOCATION IS NOT PRACTICAL DUE TO CONSTRUCTION JOINT SPACING, LONGITUDINAL RPM SPACING MAY DEVIATE BY NOT MORE THAN 10% FROM THE TYPICAL SPACING. NO DEVIATION FROM THE LATERAL PLACEMENT IS PERMITTED EXCEPT AS APPROVED BY THE ENGINEER.



Legend

(XX) Striping Detail Number

Bidirectional White/Red RPM

White — Red

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| • SIGNATURES • | | • CHECK TITLES • | |
| Drawn By: Justin Smith | Checked By: Justin Smith | Standard Lane Use Pavement Marking Details - Reflective Pavement Markers (RPMs) - One-Way Facilities | |
| Date: 01/25/20 | Approved By: Steve Smith | | |
| File: 10-1118.000 | | | |

11/26/2014

STANDARD DETAIL NO. TS-1113